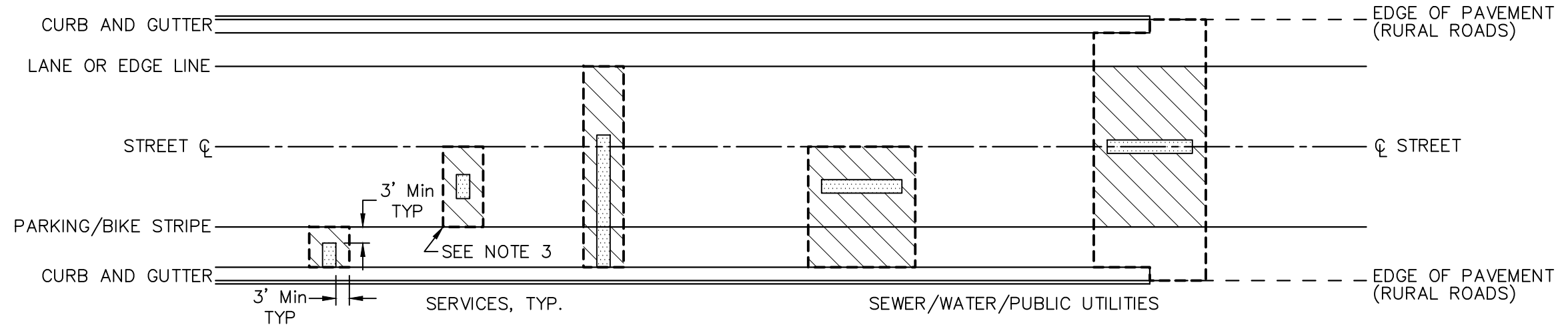


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LEGEND

REPLACEMENT LIMITS FOR PAVEMENT 5 YEARS OLD AND NEWER

WHEN PAVEMENT IS 5 YEARS OLD, OR LESS, A PAVEMENT PATCH IS REQUIRED TO THESE LIMITS. NO JOINTS OR REDUCTION IN AREA ARE ALLOWED. PAVEMENT REPLACEMENT IS REQUIRED TO THE NEXT ADJACENT CURB/EDGE OF PAVEMENT, PARKING OR LANE LINE WHENEVER A TRENCH OR DISTURBANCE OF ASPHALT OR SUPPORT MATERIAL EXTENDS BEYOND SUCH LINE. SEE NOTE 3 ON THIS PAGE.

REPLACEMENT LIMITS FOR PAVEMENT OLDER THAN 5 YEARS

WHEN PAVEMENT IS OLDER THAN 5 YEARS, A PAVEMENT PATCH IS REQUIRED TO THESE LIMITS. NO JOINTS OR REDUCTION IN AREA ARE ALLOWED. PAVEMENT REPLACEMENT IS REQUIRED TO THE NEXT ADJACENT CURB, PARKING OR LANE LINE WHENEVER A TRENCH OR DISTURBANCE OF ASPHALT OR SUPPORT MATERIAL EXTENDS BEYOND SUCH LINE. SEE NOTE 3 ON THIS PAGE.

TRENCH LIMITS AT TOP OF PAVEMENT

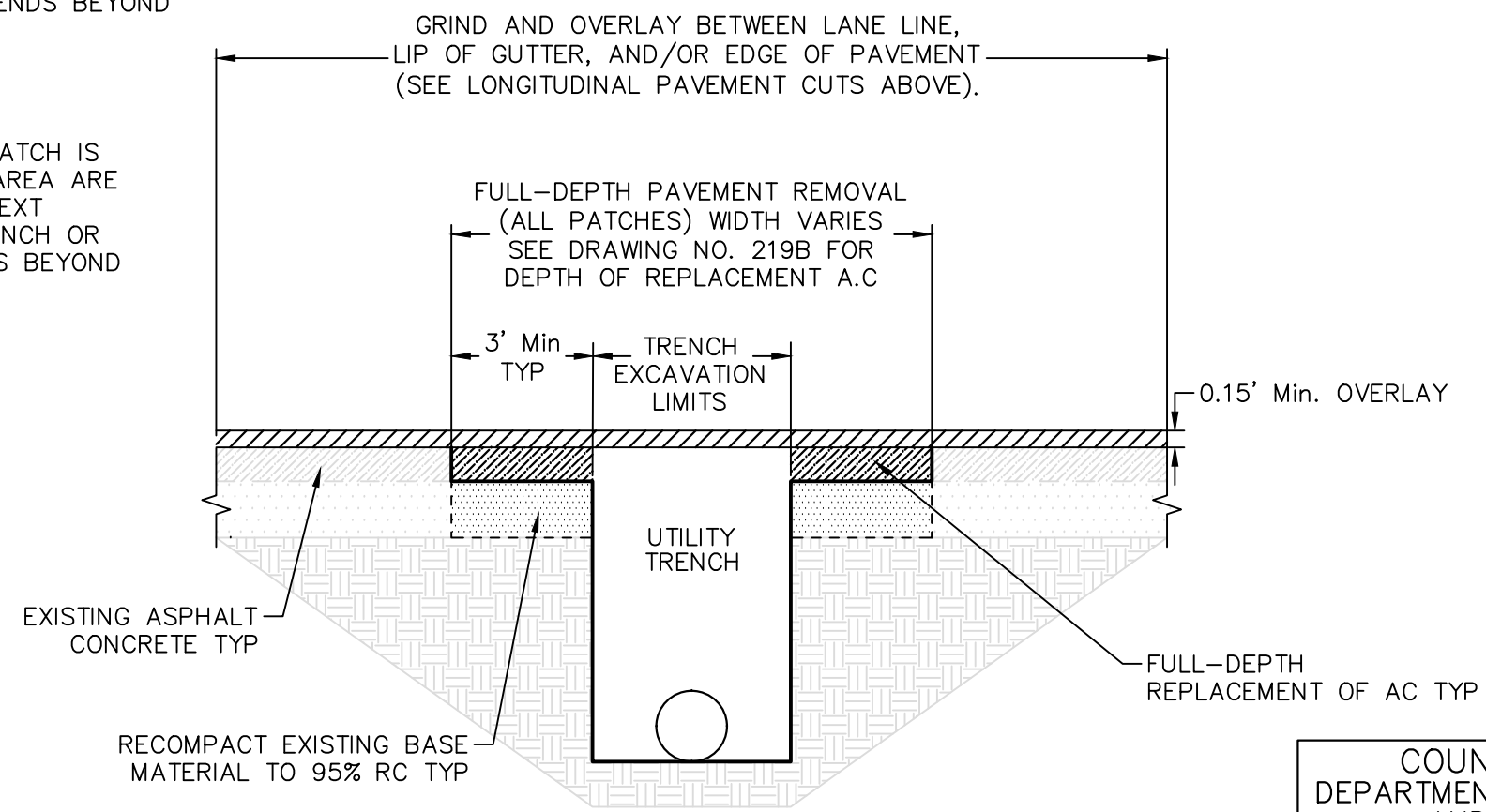
NOTES

1. FULL-DEPTH PAVEMENT PATCH JOINTS SHALL BE SEALED WITH CRAFCO POLYFLEX TYPE-1, OR EQUAL.
2. ALL CUTS SHALL BE PERPENDICULAR OR PARALLEL TO DIRECTION OF TRAVEL.
3. REMOVE ADDITIONAL PAVEMENT TO ADJACENT EXISTING PAVEMENT PATCH, MANHOLE, BOX, OR EDGE OF PAVEMENT IF THESE FEATURES ARE WITHIN 3 FEET OF FINAL SAW CUT.
4. SEE SONOMA COUNTY CONSTRUCTION STANDARD DRAWING 219E FOR REQUESTING AN EXCEPTION TO THIS STANDARD PLAN. APPROVED EXCEPTIONS SHALL BE INCLUDED AS SPECIAL ENCROACHMENT PERMIT CONDITIONS.

TYPICAL TRANSVERSE PAVEMENT CUTS

TYPICAL LONGITUDINAL PAVEMENT CUTS

PLAN



TYPICAL LIMITS OF REMOVAL

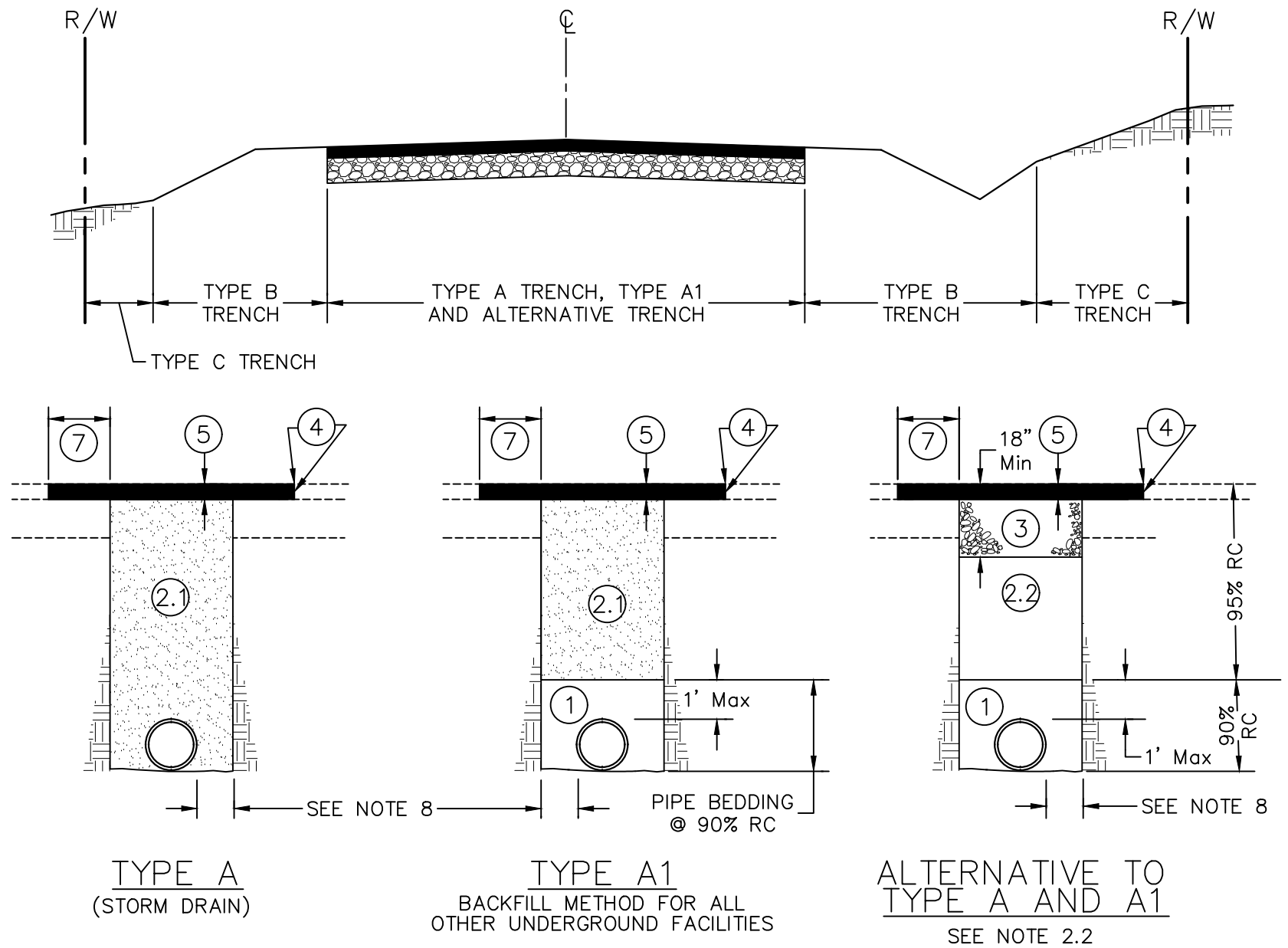
COUNTY OF SONOMA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC WORKS

PAVEMENT CUT POLICY

DATE: MAY 2013 REV. JUN. 2019	SCALE: NONE	DRAWING NO. 219A
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NOTES

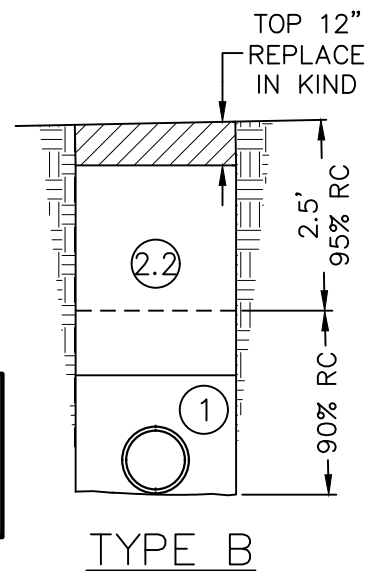
1. PIPE BEDDING REQUIREMENTS AS REQUIRED BY AGENCY INSPECTING PIPE. STORM DRAIN BEDDING PER STATE STANDARD PLANS AND SECTION 19 OF THE STATE STANDARD SPECIFICATIONS.
- 2.1 TYPE A AND A1: SLURRY CEMENT BACKFILL PER SECTION 19 OF THE STATE STANDARD SPECIFICATIONS.
- 2.2 STRUCTURE BACKFILL MATERIAL MEETING THE SPECIFICATIONS IN SECTION 19 OF THE STATE STANDARD SPECIFICATIONS MAY BE USED AT CONTRACTOR'S OPTION. IF ALTERNATIVE IS USED, CONTRACTOR WILL PROVIDE A REPORT FROM A CIVIL ENGINEER LICENSED IN THE STATE OF CALIFORNIA, CERTIFYING THAT CONTRACTOR'S WORK MEETS SPECIFICATIONS. PEA GRAVEL SHALL NOT BE USED FOR STRUCTURE BACKFILL.
3. CLASS 2 AGGREGATE BASE MATERIAL. THICKNESS SHALL BE EQUAL TO THE THICKNESS OF THE EXISTING ROAD BASE, BUT IN NO CASE SHALL THE THICKNESS BE LESS THAN 18".
4. TACK COAT S.S. 1 EMULSIFIED ASPHALT. SEAL ALL JOINTS WITH CRAFCO POLYFLEX TYPE-1, OR EQUAL, AFTER PAVING.
5. TRENCH SHALL BE PAVED WITH ASPHALT CONCRETE WHOSE THICKNESS IS EQUAL TO THE THICKNESS OF THE EXISTING PAVEMENT, BUT IN NO CASE SHALL THICKNESS BE LESS THAN THAT SHOWN IN THE TRENCH A.C. PAVING TABLE. ASPHALT CONCRETE SHALL BE 1/2" TYPE A CONFORMING TO THE PROVISIONS IN SECTION 39, ASPHALT CONCRETE, OF THE CALTRANS STANDARD SPECIFICATIONS (MAY 2006).
6. REPLACE WITH NATIVE MATERIAL RELATIVE COMPACTED TO 90%.
7. REFER TO DRAWING NO. 219A FOR DIMENSION.
8. STORM DRAIN: TWO (2) FEET; SIDE CLEARANCE MAY BE REDUCED TO A MINIMUM OF 6" WHEN SLURRY CEMENT BACKFILL IS USED. UTILITIES: TWO (2) FOOT MINIMUM TRENCH WIDTH UNLESS SLURRY BACKFILL IS USED.
9. ANY PORTLAND CEMENT CONCRETE ROADWAY REMOVED DURING EXCAVATION SHALL BE REPLACED IN KIND AND THICKNESS PER STATE STANDARD PLANS P3, LONGITUDINAL JOINT DETAIL.
10. REFERENCES MADE TO STATE STANDARD PLANS AND STANDARD SPECIFICATIONS ARE TO THE MAY 2006 (AS AMENDED) ISSUES.



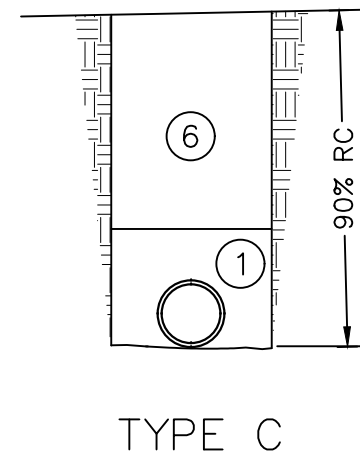
TYPE A
(STORM DRAIN)

TYPE A1
BACKFILL METHOD FOR ALL
OTHER UNDERGROUND FACILITIES

ALTERNATIVE TO
TYPE A AND A1
SEE NOTE 2.2



TYPE B



TYPE C

MINIMUM UTILITY COVER TABLE

UTILITY TYPE	Min. DEPTH OF COVER
STORM DRAIN CONDUITS	36"
SEWER SERVICE LATERALS	36"
WATER SERVICE LATERALS	36"
WATER LINE >2" DIAMETER	36"
SEWER MAIN LINES	36"
ELECTRICAL TRANSMISSION CABLES	36"
TELECOMMUNICATION CABLES	36"
GAS LINES & SERVICE LATERALS	60"

TRENCH AC PAVING TABLE

STREET TYPE	MIN. A.C. THICKNESS
RESIDENTIAL/LOCAL	0.25'
COLLECTOR	0.35'
ARTERIAL	0.45'

COUNTY OF SONOMA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC WORKS


TRENCH BACKFILL AND
PAVING DETAILS

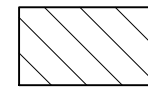
DATE: MAY 2013
REV. JUN. 2019

SCALE: NONE

DRAWING NO.
219B

LEGEND

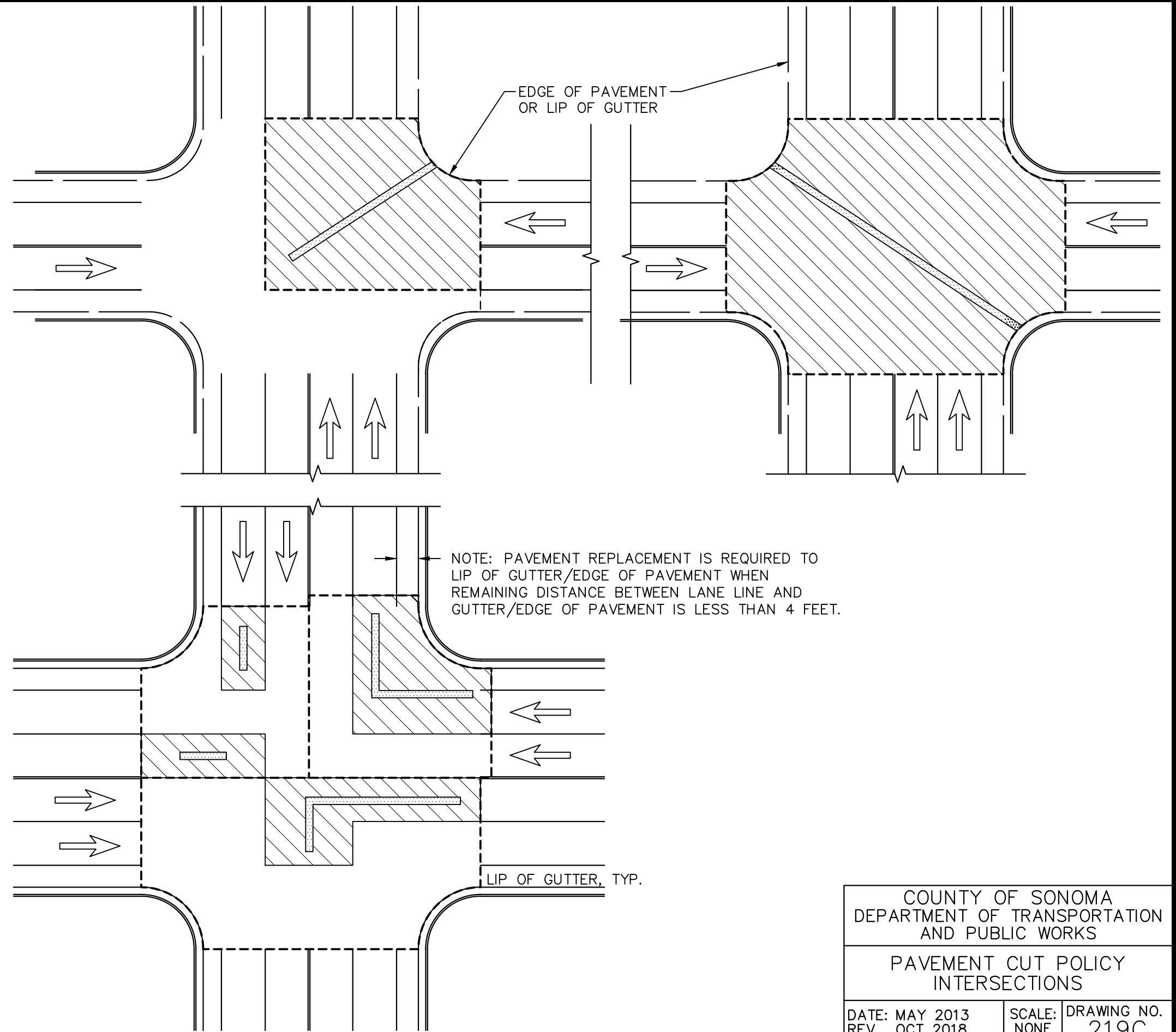
 **REPLACEMENT LIMITS FOR PAVEMENT 5 YEARS OLD AND NEWER**
 WHEN PAVEMENT IS 5 YEARS OLD, OR LESS, A PAVEMENT PATCH IS REQUIRED TO THESE LIMITS. NO JOINTS OR REDUCTION IN AREA ARE ALLOWED. PAVEMENT REPLACEMENT IS REQUIRED TO THE NEXT ADJACENT CURB/EDGE OF PAVEMENT, PARKING OR LANE LINE WHENEVER A TRENCH OR DISTURBANCE OF ASPHALT OR SUPPORT MATERIAL EXTENDS BEYOND SUCH LINE. SEE NOTE 3 ON THIS PAGE.

 **REPLACEMENT LIMITS FOR PAVEMENT OLDER THAN 5 YEARS**
 WHEN PAVEMENT IS OLDER THAN 5 YEARS, A PAVEMENT PATCH IS REQUIRED TO THESE LIMITS. NO JOINTS OR REDUCTION IN AREA ARE ALLOWED. PAVEMENT REPLACEMENT IS REQUIRED TO THE NEXT ADJACENT CURB, PARKING OR LANE LINE WHENEVER A TRENCH OR DISTURBANCE OF ASPHALT OR SUPPORT MATERIAL EXTENDS BEYOND SUCH LINE. SEE NOTE 3 ON THIS PAGE.

 **TRENCH LIMITS AT TOP OF PAVEMENT**

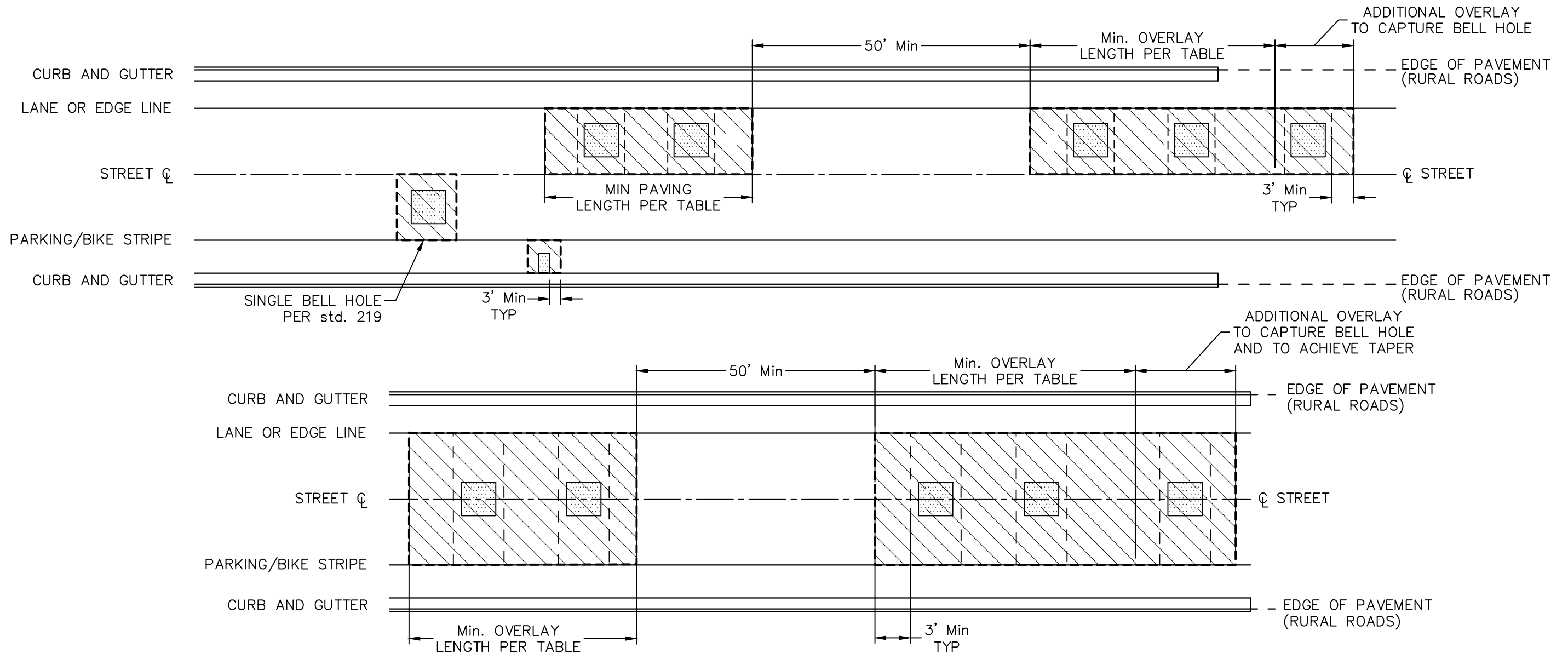
NOTES

1. FULL-DEPTH PAVEMENT PATCH JOINTS SHALL BE SEALED WITH CRAFCO POLYFLEX TYPE-1, OR EQUAL.
2. ALL CUTS SHALL BE PERPENDICULAR OR PARALLEL TO DIRECTION OF TRAVEL.
3. REMOVE ADDITIONAL PAVEMENT TO ADJACENT EXISTING PAVEMENT PATCH, MANHOLE, BOX, OR EDGE OF PAVEMENT IF THESE FEATURES ARE WITHIN 3 FEET OF FINAL SAW CUT.
4. TRAFFIC SIGNAL LOOP DETECTOR REPLACEMENT: REQUIRED PAVEMENT REMOVAL AND REPLACEMENT SHALL BE FOR THE FULL WIDTH OF THE LANE. SEE DRAWING 630 FOR ADDITIONAL DETAILS.



COUNTY OF SONOMA DEPARTMENT OF TRANSPORTATION AND PUBLIC WORKS		
PAVEMENT CUT POLICY INTERSECTIONS		
DATE: MAY 2013 REV. OCT 2018	SCALE: NONE	DRAWING NO. 219C

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NOTES

1. PATCH BELL HOLE PER STD 219A PRIOR TO OVERLAY.
2. OVERLAY SHALL HAVE A Min. THICKNESS OF 0.15 ft.
3. Min. 50 ft. SPACING BETWEEN OVERLAYS. IF LESS THAN 50 ft. SPACING, THE OVERLAY SHALL BE CONTINUED TO INCORPORATE THE GAP.
4. Min. OVERLAY LENGTH SHALL BE PER THE CALIFORNIA ROAD SYSTEM (CRS) CLASSIFICATION (MAP AVAILABLE ON THE DTPW WEBSITE):

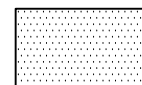
BELL HOLE OVERLAY LENGTH TABLE

CRS ROAD CLASSIFICATION	Min. OVERLAY LENGTH
OTHER PRINCIPAL ARTERIAL	300'
MINOR ARTERIAL	300'
MAJOR COLLECTOR	300'
MINOR COLLECTOR	200'
LOCAL	100'

LEGEND



OVERLAY LIMITS FOR BELL HOLES
 WHEN THERE IS MORE THAN 1 BELL HOLE WITHIN MINIMUM OVERLAY LENGTHS (SEE BELL HOLE OVERLAY LENGTH TABLE), A PAVEMENT OVERLAY IS REQUIRED TO THESE LIMITS. NO JOINTS OR REDUCTION IN AREA ARE ALLOWED. THE LENGTH OF THE PAVING OVERLAYS SHALL BE DICTATED BY THE ROAD CLASSIFICATION (SEE BELL HOLE OVERLAY LENGTH TABLE) AND BELL HOLE SPACING.



BELL HOLE LIMITS AT TOP OF PAVEMENT

COUNTY OF SONOMA DEPARTMENT OF TRANSPORTATION AND PUBLIC WORKS		
BELL HOLE POLICY		
DATE: APR 2019 REV.	SCALE: NONE	DRAWING NO. 219D

POLICY FOR EXCEPTION FROM, OR ALTERNATIVE TO, THE REQUIREMENTS OF
COUNTY TRENCH REPAIR CONSTRUCTION STANDARDS

- A. **APPLICABILITY:** THIS PROCESS SHALL ONLY BE AVAILABLE FOR COUNTY-ISSUED ENCROACHMENT PERMITS FOR THE RESTORATION OF PERMITTED EXCAVATIONS WITHIN COUNTY HIGHWAYS, PUBLIC ROADS, OR OTHER RIGHTS-OF-WAY. THIS PROCESS SHALL NOT BE AVAILABLE AS TO ANY OTHER COUNTY CONSTRUCTION STANDARD.
- B. **COMPLIANCE:** AN ENCROACHMENT PERMIT SHALL NOT BE ISSUED UNTIL THE APPLICABLE CONSTRUCTION STANDARD HAS BEEN MET OR WILL BE MET AS A CONDITION OF SUCH ISSUANCE OR GRANT OF APPROVAL, OR UNLESS THE PROJECT IS RELIEVED OF THE REQUIREMENT OR AN ALTERNATE METHOD FOR MEETING THE REQUIREMENT IS APPROVED IN ACCORDANCE WITH THIS POLICY.
- C. **APPLICATION:** AN APPLICANT SEEKING EXCEPTION FROM OR ALTERNATIVE TO A SPECIFIC REQUIREMENT OF THE CONSTRUCTION STANDARD SHALL SUBMIT A WRITTEN REQUEST FOR EXCEPTION TO THE DEPARTMENT OF TRANSPORTATION AND PUBLIC WORKS (TPW) (SEE PARAGRAPH D). SUCH REQUEST SHALL CLEARLY SET FORTH THE GROUNDS UPON WHICH THE APPLICANT FINDS THAT THE EXCEPTION OR ALTERNATIVE IS JUSTIFIED, WITH SUPPORTING DETAIL OR OTHER INFORMATION DEEMED NECESSARY BY COUNTY. EXCEPTIONS OR ALTERNATIVES MUST BE APPROVED PRIOR TO THE ISSUANCE OF AN ENCROACHMENT PERMIT.
- 4. THAT THE EXCEPTION WILL NOT RESULT IN SIGNIFICANT ADVERSE PAVEMENT IMPACTS COMPARED TO THE STRICT APPLICATION OF THE STANDARD;
- 5. THAT THE GRANTING OF THE EXCEPTION WILL NOT BE DETRIMENTAL TO THE PUBLIC HEALTH, SAFETY, CONVENIENCE AND GENERAL WELFARE OR INJURIOUS TO OTHER PROPERTY IN THE VICINITY OF THE ENCROACHMENT; AND
- 6. THAT THE REQUESTED EXCEPTION AND THE PROPOSED EXCAVATION REPAIR DESIGN ARE OTHERWISE REASONABLE.
- F. NOTWITHSTANDING ANY OF THE FOREGOING, NEITHER THIS POLICY NOR ANYTHING HEREIN SHALL LIMIT OR OTHERWISE AFFECT THE ABILITY AND DISCRETION OF THE DIRECTOR OF TPW, AS ROAD COMMISSIONER, TO REQUIRE, WITH REGARD TO ANY ENCROACHMENT, TERMS AND CONDITIONS NECESSARY FOR THE PROTECTION OF THE COUNTY HIGHWAYS, PUBLIC ROADS, OR OTHER RIGHTS-OF-WAY, OR ANY PORTION THEREOF, UNDER APPLICABLE LAWS, INCLUDING CALIFORNIA STREETS AND HIGHWAYS CODE SECTION 1462 AND SONOMA COUNTY MUNICIPAL CODE SECTION 15-9.

EXCLUSIONS: EXCEPTIONS OR ALTERNATIVES WILL NOT BE CONSIDERED FOR EXCAVATIONS IN THE FOLLOWING:

- 1. COUNTY-CLASSIFIED ARTERIAL AND COLLECTOR ROADS;
- 2. ANY ROAD HAVING A SURFACE TREATMENT THAT IS LESS THAN FIVE (5) YEARS OLD;
- 3. SEGMENTS OF ROADS AND STREETS HAVING AN ENGINEERED STRUCTURAL SECTION AND A PAVEMENT CONDITION INDEX (PCI) GREATER THAN 25.
- D. **PROCESSING:** APPLICATIONS FOR EXCEPTION OR ALTERNATIVE SHALL BE SUBMITTED TO THE TPW PRIOR TO SUBMITTAL OF PLANS TO THE ENCROACHMENT SECTION AT PERMIT SONOMA (PRMD). APPLICATIONS MAY BE SUBJECT TO PAYMENT OF APPLICABLE FEES. THE AUTHORITY TO APPROVE, APPROVE WITH CONDITIONS OR DENY THE APPLICATION SHALL BE AT THE DISCRETION OF THE DIRECTOR OF TPW (OR DESIGNEE), WHOSE DECISION SHALL BE FINAL.
- E. **FINDINGS FOR APPROVAL:** NO EXCEPTION OR ALTERNATIVE MAY BE APPROVED UNLESS THE FOLLOWING ARE DETERMINED TO THE SATISFACTION OF THE DIRECTOR OF TPW:
 - 1. THAT THE STRICT APPLICATION OF THE STANDARD WILL, DUE TO SPECIAL CIRCUMSTANCES OR CONDITIONS, CREATE AN UNDUE HARDSHIP OR WOULD BE OTHERWISE EXCESSIVE;
 - 2. THAT THE PROPOSED PAVEMENT REPAIR IS SUITABLE FOR THE PRESERVATION OF THE ADJACENT PAVEMENT;
 - 3. THAT, IF AN ALTERNATIVE IS PROPOSED, THE DESIGN, MATERIAL, METHOD OR WORK PROPOSED IS, FOR THE PURPOSE INTENDED, EQUIVALENT OR SUPERIOR TO THE STANDARD;

COUNTY OF SONOMA DEPARTMENT OF TRANSPORTATION AND PUBLIC WORKS		
EXCEPTION TO STANDARD DETAILS		
DATE: NOV 2018 REV.	SCALE: NONE	DRAWING NO. 219E